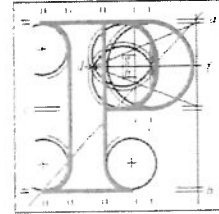


Our Case Number: ABP-320164-24

Your Reference: J Murphy Construction Limited - SID-OBS-00



**An
Bord
Pleanála**

BMA Planning
c/o Ray Ryan
Taney Hall
Eglinton Terrace
Dundrum
Dublin 14
D14 C7F7

Date: 29 October 2024

Re: DART + Coastal North Railway Order 2024 - Northern Line between Dublin City Centre and Drogheda including the Howth Branch
Dublin City Centre and Drogheda, located in counties Dublin, Meath and Louth

Dear Sir / Madam,

An Bord Pleanála has received your recent letter in relation to the above mentioned case. The contents of your letter have been noted.

Please be advised that as J Murphy Construction Limited appears to be listed on the compulsory purchase order schedule which accompanied this application, therefore a refund will be made to the credit/debit card used to make the online observation.

More detailed information in relation to strategic infrastructure development can be viewed on the Board's website: www.pleanala.ie.

If you have any queries in relation to the matter please contact the undersigned officer of the Board at laps@pleanala.ie

Please quote the above mentioned An Bord Pleanála reference number in any correspondence or telephone contact with the Board.

Yours faithfully,

Aisling Reilly
Executive Officer
Direct Line: 01-8737131

RA03

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17/10/2024

Project: DART+ Coastal North – ABP-320164-24
Site: Lands at Newtown, Drogheda (MacBride Station)
Client: J Murphy Construction Limited / Ravala Limited

Dear Sir/ Madam,

On behalf of J Murphy Construction Limited / Ravala Limited, we have been instructed to make the following submission to the proposed Railway Order for the Dart+ North Electrified Heavy Railway (Bord Ref. ABP-320164-24). The fee (€50) was paid by via the online portal.

We trust our clients' concerns will be taken in consideration by An Bord Pleanala in the assessment of the proposed Railway Order for the Dart+ Coastal North Project and look forward to hearing from you in due course

Yours sincerely,



Ray Ryan
BMA PLANNING
ray@bmaplanning.ie

Project: DART+ Coastal North – ABP-320164-24
Site: Lands at Newtown, Drogheda (MacBride Station)
Client: J Murphy Construction Limited / Ravala Limited



Submission to An Bord Pleanála re DART+ Coastal North – Drogheda (MacBride Station)

October 2024

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1.0 INTRODUCTION

1.1 Purpose of Submission

This submission to the Dart+ Coastal North Railway Order has been prepared on behalf of our clients:

J Murphy Developments Limited
The Bryanstown Centre,
Drogheda
Co. Louth

Ravala Limited
22A Boyne Business Park
Greenhill
Drogheda
Co. Louth

The submission relates to the potential impacts of the project on our clients' landholding at Newtown, Drogheda. The lands are described in Section 2 below.

We have reviewed the drawings and details submitted as well as the reports, including the Environmental Impact Assessment Report (EIAR).

The submission has been prepared in conjunction with Waterman Moylan Consulting Engineers who have provided technical advice in relation to the above elements.

1.2 Subject Lands

The are within the geographical area – Zone E – Drogheda MacBride station and surrounds.

The subject lands are located at Newtown, Drogheda immediately north of the Drogheda Train Station (MacBride Station) which is the proposed terminus of the future DART+ Coastal North project.

The lands outlined red on the aerial view below (Fig. 1) comprise c. 11.3ha and are owned as follows:

- A. J Murphy Construction Limited (c.8.1ha)
- B. Ravala (3.2ha)

Our clients constructed the recently completed 'Newtown View' residential development and as part of that development, a new road from Marsh Road (R150) has been constructed.



Fig 1: Subject Lands

1.3 Overview of Railway Order Proposals for Subject Lands / Outcome Sought

The submission deals with our clients' concerns in relation to aspects of the proposals and CPO in relation to permanent and temporary land take as outlined below.

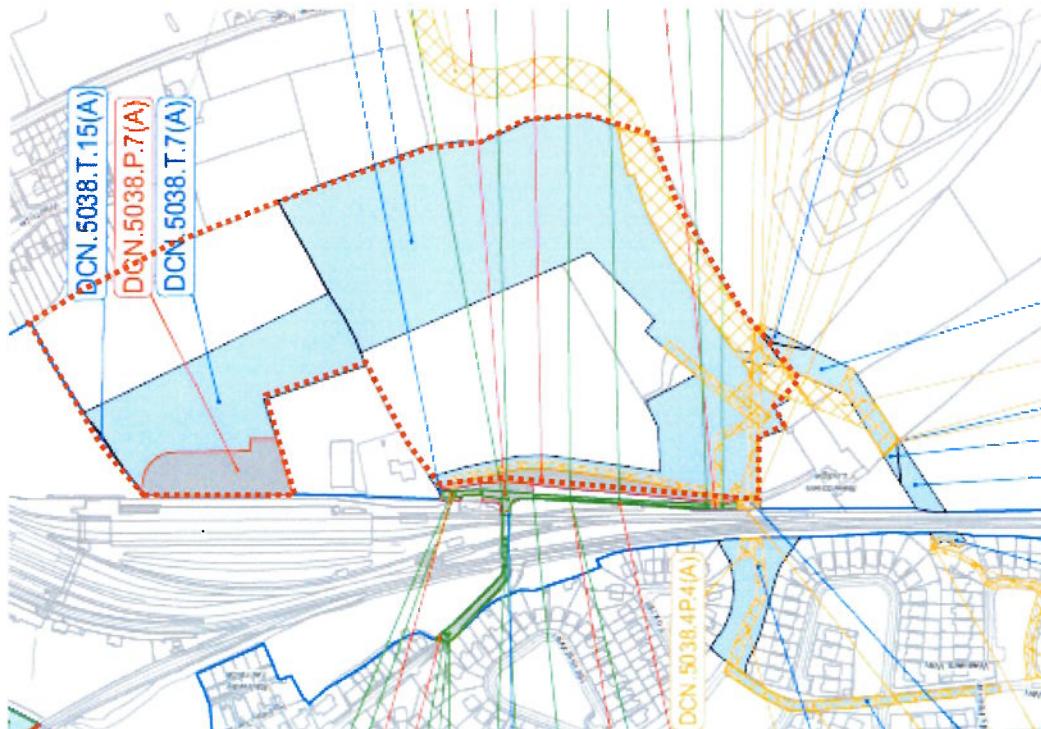


Fig 2: Subject Lands – Approx Extent of CPO (Extract from Property Plan No. 38)

Permanent Acquisition

These lands totalling (0.57ha – see below) will be acquired as part of the CPO process and ownership transferred to CIE. The two main elements are as follows and are identified in Fig.3 below:

1. Reconstruction Of The Overbridge OBB 080 At McGraths Lane.
2. Construction Of A New Traction Substation At MacBride Station

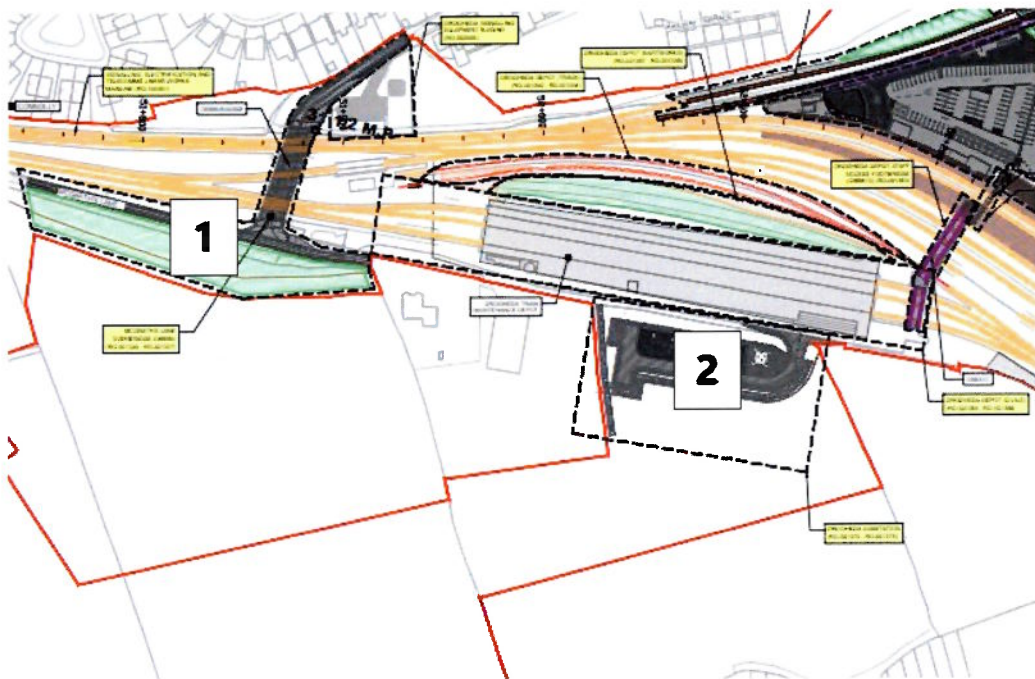


Fig 3: Extract from Drogheda MacBride Works Layout Plan

The drawings and particulars submitted identify two elements on our clients lands and the Second Schedule (Part 1) provides the details as summarised in Table 1 below

Table 1: Summary of Proposed Permanent Acquisition

RO Works	Owner	Quantity sqm
Reconstruction Of The Overbridge OBB 080 At McGraths Lane. (McGrath’s Lane Overbridge (RO-001330 – RO-001337))	J Murphy	1909.91sqm
Construction Of A New Traction Substation At MacBride Station (Drogheda Substation (RO-001370 – RO-001373))	Ravala	3,806.43sqm
Total		5,716.34sqm

Amendments are proposed in this submission to address our clients' concerns in relation to these elements to align with the future development proposals for the lands.

Temporary Acquisition

in addition it is proposed that a large proportion of our clients lands (c. **5.66ha** – see below) are to be temporarily acquired to facilitate a construction compound adjacent to McBride railway station.

The Third Schedule provides the following details

Ref	Owner	Area
Ref No: 5038.T.1(A)	J Murphy	43,827.77 sqm.
Ref No: 5038.T.7(A)	Ravala	12,822.61sqm
Total		56,650.38 sqm

Permanent and Temporary Rights of Ways/ Easements

We note the proposed rights of way and other easements sought but for the purpose of this submission we have focused on the primary works which are most relevant. Refer to Appendix A.

2.0 PLANNING OVERVIEW

2.1 Development Plan / Zoning Context

The subject lands are zoned in the current *Louth County Development Plan 2021-2027* as '**J1 Transportation Development Hub**'. This zoning includes the following objective : '*To support the development of a public transport hub commensurate with a transportation hub*'. See Fig 4 below

The Development Plan further states that '*this zoning objective will facilitate the development of a public transportation hub and is suitable for other land use including high-density residential development and retail and office uses*'

This above zoning objective clearly indicate that the subject lands are an important strategic development site for Drogheda and an integral part of the land use strategy that is underpinned by the DART+ Coastal North project.

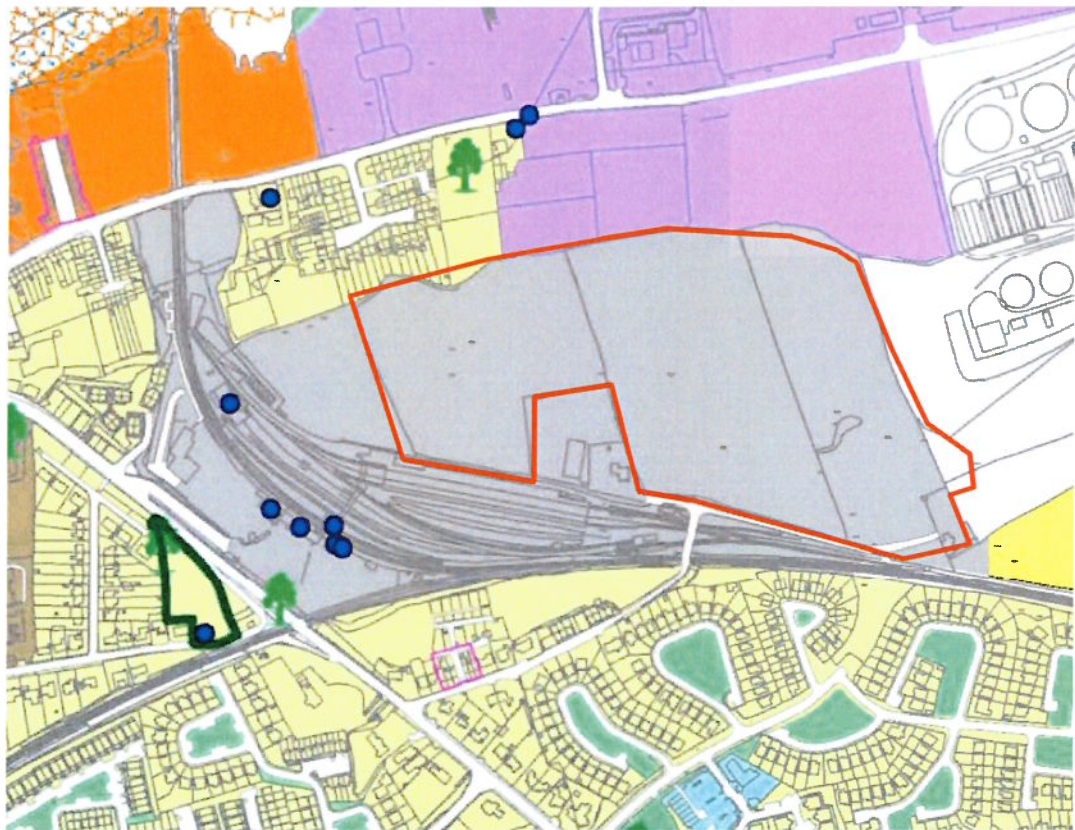


Fig 4: Louth County Development Plan 2022-2028 – Zoning Objective

2.2 Recent Planning History

Our clients have commenced construction on the Newtown lands and have completed the Newtown View (133 houses for Respond permitted under PA reg ref 17-387 – under construction) development to the immediate east of the subject lands.

This development included the access road off Marsh Road which is now complete and which is relied upon to access the construction compounds included in the Railway Order application.

A Strategic Housing Development (SHD) application on the eastern two thirds of the subject lands (J Murphy Developments Limited) was granted permission in 2019 under Bord Ref: TA15.305110. <https://www.pleanala.ie/en-ie/case/305110> . This development included the following:

Applicant:	Ravala Limited
Description:	450. no residential units (81 no. houses and 369 no. apartments), creche, neighbourhood centre, offices and associated site works. Newtown, Marsh Road & McGraths Lane Railway Terrace, Drogheda, Co. Louth.
Decision:	Granted 27/11/2019

The Board's Decision was quashed by Order of the High Court on 21/08/2020 on procedural grounds and so the development was delayed and another planning application will be made.

A subsequent planning application for 68 houses and site development works was withdrawn (Reg Ref: 211333 ; Bord Ref: PL15.315291 Murlyn (Investments) Limited (Applicant). That development was deemed by the planning authority to be too low density for a strategic site in close proximity to a public transport node. In addition to citing the Louth County Development Plan 2021 - 2027 (policy objective HOU15), the planning authority's decision cited National Policy Objective 35 of the National Planning Framework Project Ireland 2040 which promotes higher sustainable densities, compact growth, the consolidation of urban areas based on sustainable transport principles.

2.3 National Policy/ Guidelines

As outlined above national policy promotes higher densities based around public transport accessibility on the subject lands are a prime example on the site within a large urban centre capable of accommodating the high density residential development.

These general policies have been supplemented by specific guidance published by government in January 2024. The guidance entitled ‘*Sustainable Residential Development And Compact Settlements –Guidelines For Planning Authorities 2024*’ were published in January 2024 in response to the National Planning Framework.

To achieve compact growth, the Guidelines state that it will be necessary to increase the scale of new buildings in all parts of our cities and towns, with highest densities at the most central and accessible locations, particularly in city centres and close to public transport nodes and interchanges. The Guidelines are implemented by establishing suitable residential density ranges based on settlement size and area type and having regard to accessibility to public transport. Section 3.0 of the Guidelines provides a methodology for establishing residential density based on settlement and area types and having regard to accessibility and local character. Tables 3.1 to 3.7 of the Guidelines identify five settlements and four sub-areas to which density ranges (dwelling per hectare – “dph”) are applied. These are summarised in Table 1 below.

Table 1 Settlements, Area types and Density Ranges (Net)

Settlement	Area 1	Area 2	Area 3	Area 4
	Centre	Urban Neighbourhoods	Suburban/Urban Extension/ Edge	Village
1. Cities and Metro.				
Dublin & Cork	100 – 300 dph	50 – 250 dph	40 – 150 dph	-
Limerick, Galway & Waterford	100 – 250 dph	50 – 200 dph	35 –100 dph at ‘accessible’ location	-
Metro Towns & Villages	50 – 150 dph		Up to 100 dph	Not below 25dph
2. Regional Growth Centres	50 – 150 dph		35 – 100 dph	-
3. Key Towns & Large Towns	40 – 100 dph		30 – 80 dph	-
4. Small & Medium Towns	Respond to Established Context		25 – 40 dph	-
5. Rural Towns & Villages	Respond to Established Context			

Regarding accessibility, Table 3.8 of the Guidelines identifies four categories of “accessibility”, with the highest densities directed to sites classified as ‘**High Capacity Public Transport Node or Interchange**’, as follows:

High Capacity Public Transport Node or Interchange

- *Lands within 1,000 metres (1km) walking distance of an existing or planned high capacity urban public transport node or interchange, namely an interchange or node that includes DART, high frequency Commuter Rail, light rail or MetroLink services; or locations within 500 metres walking distance of an existing or planned BusConnects 'Core Bus Corridor'¹² stop.*
- *Highest densities should be applied at the node or interchange and decrease with distance.*
- *'Planned public transport' in these Guidelines refers to transport infrastructure and services identified in a Metropolitan Area Transport Strategy for the five cities and where a public authority (e.g. National Transport Authority, Transport Infrastructure Ireland or Irish Rail) has published the preferred route option and stop locations for the planned public transport.*

(our emphasis added)

Louth County Council in partnership with Meath County Council has commenced the preparation of a Joint Local Area Plan for Drogheda. This Plan will set out a land use strategy for the future growth and sustainable development of Drogheda and will have regard to the DART+ Coastal North proposals and the most recent guidelines referred to above.

2.4 Conclusion

Based on the planning history of the site and recent planning policy guidelines outlined above, the subject lands which are centrally located in Drogheda – A Regional Growth Centre – would be suitable for development at up to 150 dwellings per hectare.

Our client is currently in the process of preparing a masterplan for a phased development of the overall 11.3ha site in accordance with a masterplan which has capacity to deliver over 1500 residential units on the subject lands in accordance with the 2024 Ministerial Guidelines.

3.0 RECONSTRUCTION OF OVERBRIDGE AT McGRATHS LANE

3.1 Permanent Land Take - Reconstruction Of The Overbridge OBB 080 At McGraths Lane.

As outlined above, the permanent works to the bridge involve the acquisition of our clients lands along the northern edge of McGrath's Lane

Table 4-7 Summary of Bridge and Civil Structures lists the works as

Zone E OBB80/80A/80B- Railway Terrace – Bridge Replacement

The EIAR describes the works as follows:

4.10.4.1 Overbridge OBB80/80A/80B Modifications (carrying Railway Terrace)

The existing bridges (IE reference OBB80/80A/80B) are located on the southern approach to Drogheda MacBride Station (approximate chainage 51+880). The bridges are aligned to form a three-span crossing of the railway and provide vehicular access from Railway Terrace to McGrath's Lane.

.....

*It is proposed to demolish and replace these bridges (OBB80/80A/80B) with a new higher three-span bridge. (see Image 4-68). The bridge will be 5.8m wide, measured between parapet faces and will have an overall length of approximately 48m. The superstructure will comprise prestressed concrete beams with a cast-in-situ deck slab, and will be made integral with the substructure below, avoiding the need for bearings. **The bridge will be supported on piled foundations at each of the piers. The wingwalls on approach to the bridge on either side require retaining walls.***

*The existing bridge width measures approximately 5.8m between parapets. It is proposed to provide a similar width passage while complying with design standards and catering for pedestrian and cycle passage. Hence, the proposed new bridge cross section provides a 4.8m shared carriageway with a 0.5m raised verge on either side. The new bridge will have raised parapet walls with a minimum height of 1.8m above the adjacent footway. In addition, retaining walls are required at the run-off of each span and either side of the bridge (see Image 4-69). These reinforced concrete walls will run either side of the bridge on the South side of the McGrath's Lane (rail side), and either side of the Railway Terrace on the approach to the bridge. **Along the north side of McGraths Lane, an earthworks embankment will support the revised road.***

To achieve the necessary vertical clearance for the electrification of the track, the level of the new bridge (+37.10mOD) will be set up to 1.2m higher compared to the existing bridge (+35.93mOD). This will require the approach roads to be re-graded to meet the new bridge levels. Road works to the southern approach will extend approximately 70m to tie in with Railway Terrace, raising the road level at the interface with the bridge 1.17m and increasing the road gradient by 1%. **The northern extent of regrade will require roadworks to McGrath's Lane for approximately 160m, with the level of the new road (+36.43mOD) being set 2.75m higher compared to the existing levels (+33.68mOD), located at the interface with the new bridge. To limit the extent of retaining wall structures, an earthworks embankment is proposed to the north.** Elsewhere, retaining walls will contain the road approaches. Appropriate landscaping will be provided following the regrading of the road.

(bold and underlining is our emphasis)

Image 4-69 below from the EIAR shows the proposed retaining walls on McGrath's Lane and Railway Terrace and on the northern edge of McGrath's Lane shows the area of land to be acquired to facilitate an embankment cross hatched.

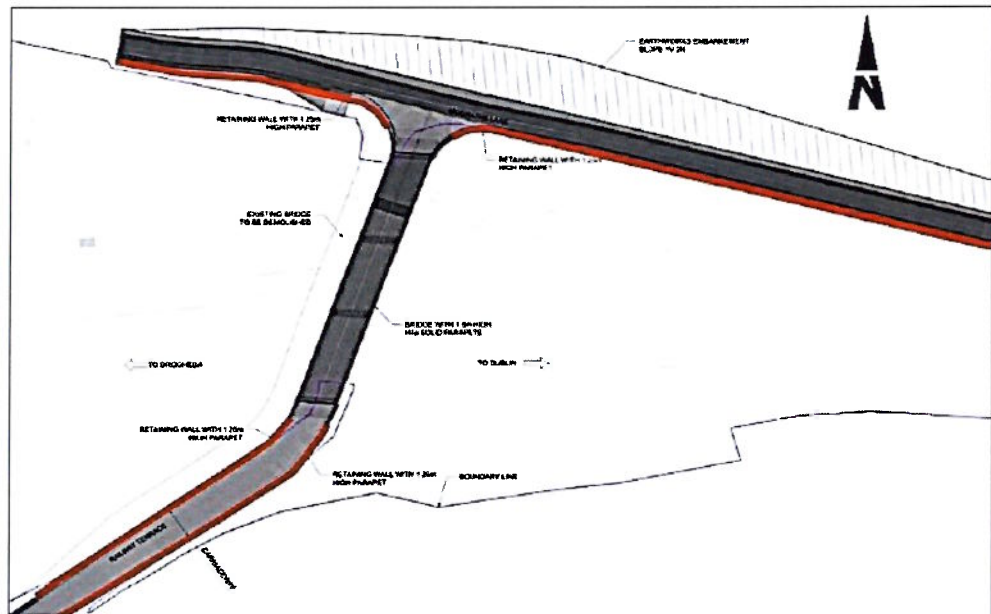


Image 4-69 Overbridge OBB80 Extents of retaining walls (red lines)

Fig 5: Extract from EIAR Image 4-69

3.2 Impact of Permanent Land Acquisition and Suggested Amendments

The impact of the bridge reconstruction and the associated increase in ground levels on the future development of our clients lands is significant and will result in the inefficient and unsustainable use of prime urban land. This impact should be mitigated by reduction of embankments and use of retaining walls.

It appears based on *EIAR Volume 2: Chapter 4 Description of Proposed Development Page 39 (Section 4.5.7.4 New Retaining Wall Structures)* that the primary factors that impact on the choice of wall include site constraints, applied loads, existing ground conditions, retained wall height, constructability and whether it retains cut or fill. Specific retaining wall solutions are described in Sections 4.6 to 4.10. We submit that the implications for the future development of the adjacent lands should have been considered in this regard.

Chapter 3 of the EIAR entitled '*3.5.3 Works Around Drogheda MacBride Station*' includes consideration of options around '*OBB80/80A/ 80B Access via Railway Terrace*'. However, there is no evidence that the option of reducing the land acquisition through retaining wall was considered.

The subject lands are designated for high density urban development and therefore should not be treated as would agricultural land or land in a suburban setting. The EIAR consideration of "Alternatives" should have addressed the impacts of the embankment on material assets (ie. land) which in this case will constitute a significant adverse impact on achievement of planning objectives for prime urban land and will result in an unsustainable outcome when compared with the alternative now suggested. Refer to sketch enclosed (Fig 6) below.

Reconstruction of Overbridge at McGrath's Lane - Outcome Sought:

The Railway Order application does not adequately accommodate the future development of the J1 'J1 (Transportation Development Hub)' lands at Newtown. The proposed use of embankments results in unnecessary acquisition of prime development land adjacent to a railway station node and is unsustainable and contrary to national and local planning policy. The Railway Order proposals at this location should be adjusted to include retaining walls (preferably a piled wall) to enable our client to construct the necessary high density development up to the retaining wall and to facilitate future cycle/ pedestrian connection to the upgraded McGrath's Lane/ Railway Terrace. Refer to sketch enclosed.

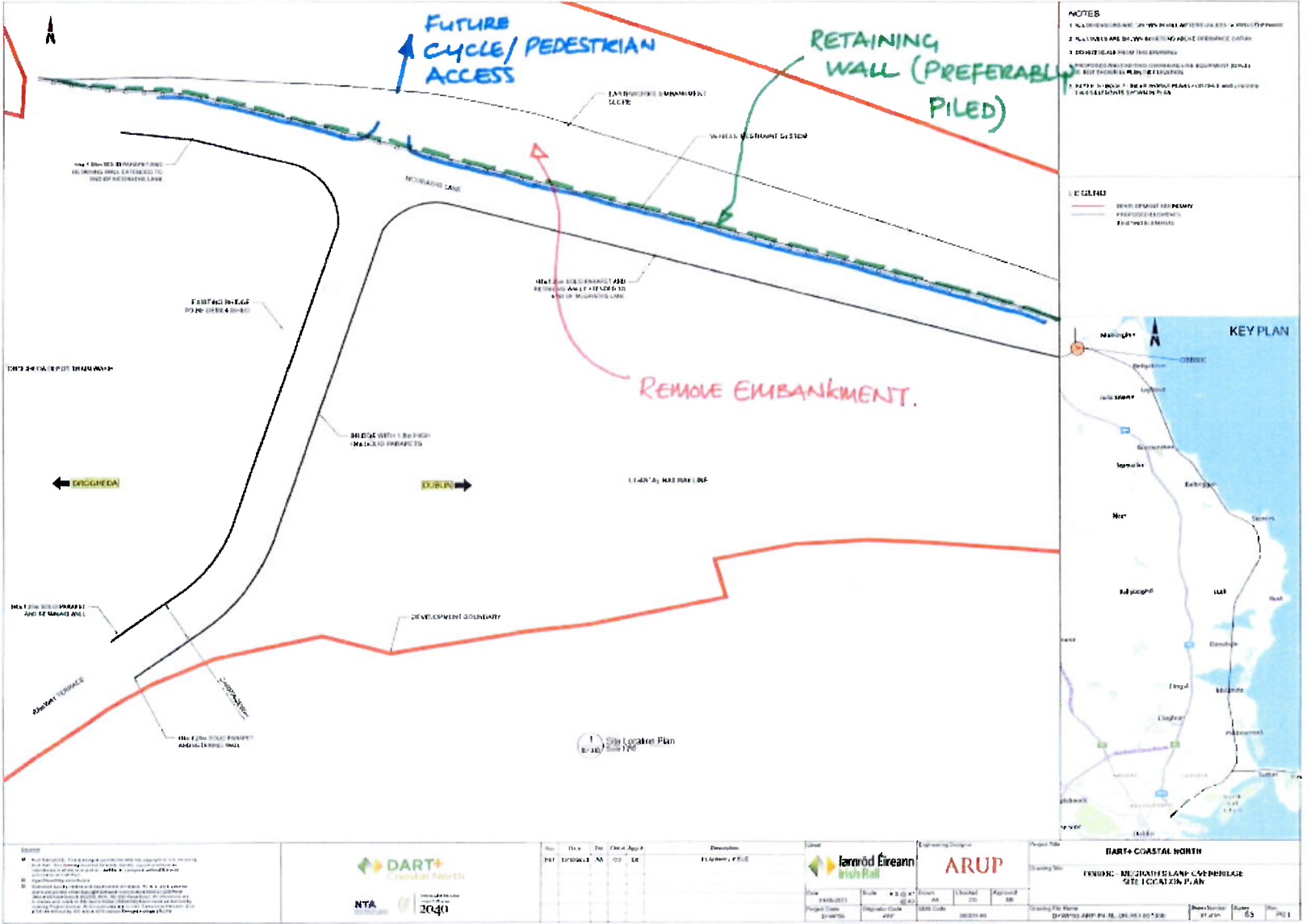


Fig 6: Illustration of Impact of Acquisition and Suggested Amendments

4.0 SUBSTATION COMPOUND AT MACBRIDE STATION

4.1 Permanent Land Take - Construction Of A New Traction Substation At MacBride Station

Chapter 3, Section 3.5.2 ('Installation of power supply substations and electricity feeding infrastructure') notes that findings from a traction power study has indicated that eight new substations will be required and identifies that one such location is Drogheda (page 38).

As outlined in the EIAR Section 4.5.2.1 'Substations', the substation buildings are divided into two parts, shared between IÉ and ESB. IÉ will have the largest part of the building (approximately 5.0m high x 30m long x 11m wide) and ESB will have the smaller part of the building (approximately 5.0m high x 16m long x 10m wide).

The Drogheda substation is described in Section 4.10.5.1 as follows:

4.10.5.1 Drogheda Substation

The location of the proposed Drogheda Substation is immediately north of the existing Drogheda Depot and within 0.2km of Drogheda MacBride Station. Access will be provided via a 5m wide road which will connect to the existing Drogheda Depot access road.

The finish proposed for the substation is a cement rendered finish on all elevations with pressed metal roof sheeting, keeping the same architectural finishes as the existing Iarnród Éireann substations. IÉ green palisade fencing will surround the site providing security, with a planted hedgerow providing screening from the north and west side of the site.

The proposed site location is not within the existing IÉ property boundary and therefore acquisition of third-party land will be necessary to accommodate the substation. *The site is bounded by Drogheda Depot access road to the south **and agricultural land to the north, east and west.***

*The existing terrain is uneven and will require earthworks, including a section of the existing gabion wall (which runs alongside the existing access road at the side of the depot supporting a 2-3m level difference) to be removed, to create an entrance point for the proposed access road. **A concrete retaining wall, up to 3.5m in height is proposed to support the terrain between the access road and proposed surface water infiltration pond (soakaway). The substation will sit at a ground level of approximately +33.00mOD with the entrance to the connecting road at a level of approximately +29.70mOD.***

An assessment of the soil maps of the area and geotechnical investigations suggest the soils are suitable for infiltration into the sub-soils. The design for surface water drainage therefore proposes that run-off will dissipate through infiltration ponds into the soils below.

*A very small area of the site will discharge into the existing surface water drainage present in the depot. There is no new foul drainage proposed for the site, given the proximity of welfare facilities within the current depot.
Lighting for the site will be provided by lighting fixed to the building. This will only be activated when the building is in use.*

(bold and underlining is our emphasis)

The EIA (Alternatives) Section 3.5.2 notes that the siting of each substation has considered the following:

3.5.2 Installation of power supply substations and electrical feeding infrastructure

...

The siting of each substation within any general area has considered the following:

- **The land-use and development context of potential locations;**
- *The substations will be located adjacent to the railway line in the form of a fenced compound surrounding a single storey building which will house all the necessary electrical switching and feeding equipment;*
- *The substations will be connected to the local power distribution network and the OHLE system using insulated cables. These cables will be installed in buried routes for additional protection;*
- *The substations will need to be accessible from the local road network for construction and maintenance purposes; and*
- *The footprint of each substation compound and requirement for the building to house the electrical equipment for both IÉ and ESB.*

While every effort has been made to contain the necessary works, including the provision of eight additional substations, within existing IÉ owned lands, this has not always been possible. *Where works are required outside of IÉ lands, lands required for Construction Compounds will typically be on a temporary basis **while the lands required for the substations will be on a permanent acquisition basis.***

The alternatives considered for each of the substation locations are detailed in the sections below.

(bold and underlining is our emphasis)

9no. options, excluding the 'Do-Nothing' option, were identified for the study area which extended from the end of McGrath's Lane to the Marsh Road Pay & Display car park and the options are identified in Image 3-22 (from EIAR page 53) below

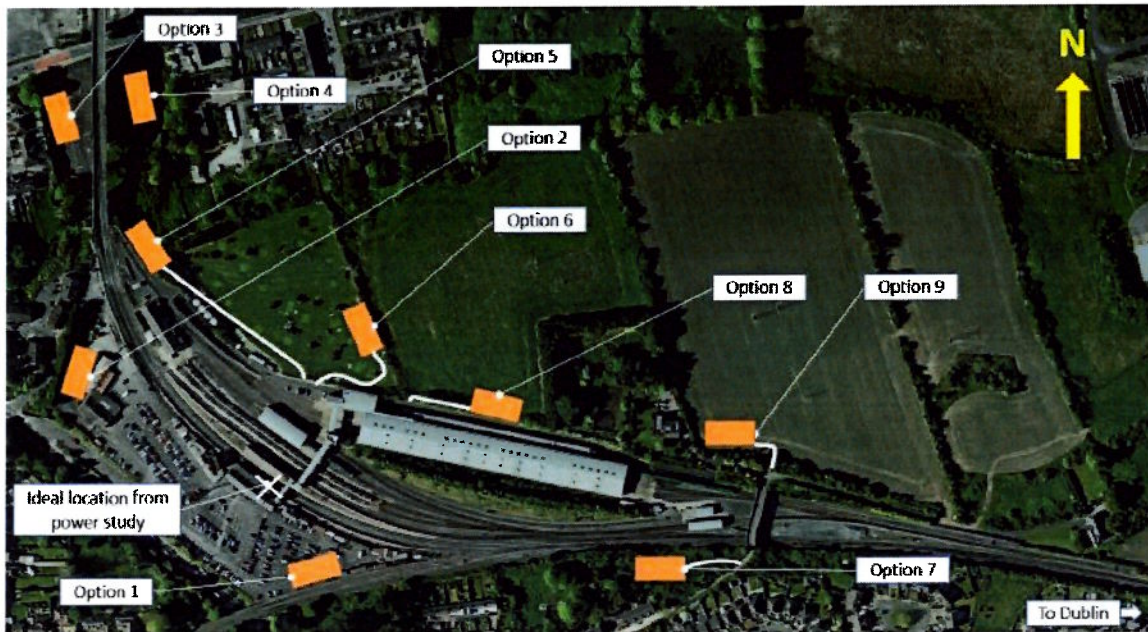


Fig 7: Mac Bride Station ESB Substation Alternative Locations Considered in EIAR

As outlined below, our client considers that the justification and rationale for the permanent acquisition to accommodate the ESB Substation is inadequate and does not support the acquisition of our clients' lands as part of the Railway Order.

4.2 Impact of Permanent Land Acquisition and Suggested Amendments

Our clients' object to the proposed substation on their lands and note the following points:

1. **Unsustainable and Unnecessary Removal of Prime Urban Land** – The proposed substation at this location results in unnecessary acquisition of prime development land adjacent to a railway station node and is unsustainable and contrary to national and local planning policy.

2. **Significant Impact on Retained Lands** – The form and design of the substation compound is incompatible with the future development of the western portion of the lands for high density urban development adjacent to a railway station. In addition to the lands to be acquired, there is a very significant impact on the retained lands as a result of the retaining walls c.3.5 metres above the existing ground level which will very severely impact on the type and extent of development permissible at this location when compared to the “no-scheme world”. There is obvious inconsistency in the approach of the acquiring authority in relation to the rationale given in the context of McGrath's Lane which proposed earth embankments '*to limit the extent of retaining wall structures.*'. (EIAR Section 4.10.4.1) and the approach in this instance.

3. **Inadequate Assessment of Alternatives** – Our clients submit that the consideration of “Alternatives” in the EIAR is inadequate insofar as the substation compound is concerned. The EIAR Table 3-30 ‘*Summary Of Longlist Sifting For Drogheda Substation*’ ruled out 3 options as “Fail” and others, including Option 8, as “Pass”. Based on the multi-criteria analysis (MCA) Table 3-31 is a ‘*Summary of MCA*’ and identifies Option 8 as the “Preferred Option”. However, no further information is provided and no analysis or details other than “*Meets project objectives and requirements*”. For example, the economic assessment of Options 1 and 6 does not appear to have considered the fact that these are located on Irish Rail lands and therefore would require the acquisition of prime high density development land at this location. Furthermore, other options would avoid the need to construct the significant new dedicated access road/turning head to the proposed sub station.

Substation Compound at MacBride Station - Outcome Sought:

- **The proposed substation should be relocated to other lands which are more suitable. The assessment of alternative locations for the ESB Substation compound is inadequate and does not support the decision to permanently acquire the subject lands for this purpose**
- **The footprint and the associated land take of the substation compound is excessive and represents an inefficient use of prime development land adjacent to a railway station node suitable for high density development. Furthermore, the location and design of the substation compound will give rise to significant injurious affection on the retained lands in the vicinity of the proposed ESB station.**

5.0 PEDESTRIAN AND CYCLE CONNECTION TO MACBRIDE STATION

The issue of future pedestrian and cycle connections to the Railway Station is also important and the Railway Order should ensure that the bridge to the station (referred to as ‘Drogheda Depot staff access footbridge (OBB81C) (RO-001360)’] is suitable and capable of being utilised as a public footbridge to connect the future development lands adjacent to the platform as denoted with the red arrow below.

This permeability is particularly required where the transport hub is integral to the “J1” zoning objective for the lands which supports the development of a high density residential scheme (1500+ units) at a public transport hub / interchange in accordance with the *Louth County Development Plan 2021-2027* and various national, regional and local planning policy sources and guidelines that are referred to in the applicants Railway Order application to An Bord Pleanála.

For our clients’ masterplanning and development strategy deliberations, it is essential that the intentions of the major stakeholder in MacBride Station are appropriately flagged and anticipated in this Railway Order application. This requires further consideration and clarity and is relevant, not only to the “J1” lands, but also to other residential and employment lands south of Marsh Road which could also access the railway station from this direction if such a facility was provided for.

As presented in this Railway Order application, the bridge (see photograph below) is referred to as a staff only facility and it appears as though no pedestrian or cycle permeability is envisaged. If that is the case, the case for the project is seriously undermined. The DART+ Coastal North project should be required to clearly address this matter and to ensure that the connections to the adjacent “J1” lands and other existing and future development lands are provided insofar as possible. In this particular case, CIE owns the lands immediately north of the footbridge at the pitch and putt club and appears to be the sole party with control over this connection.



Existing Footbridge

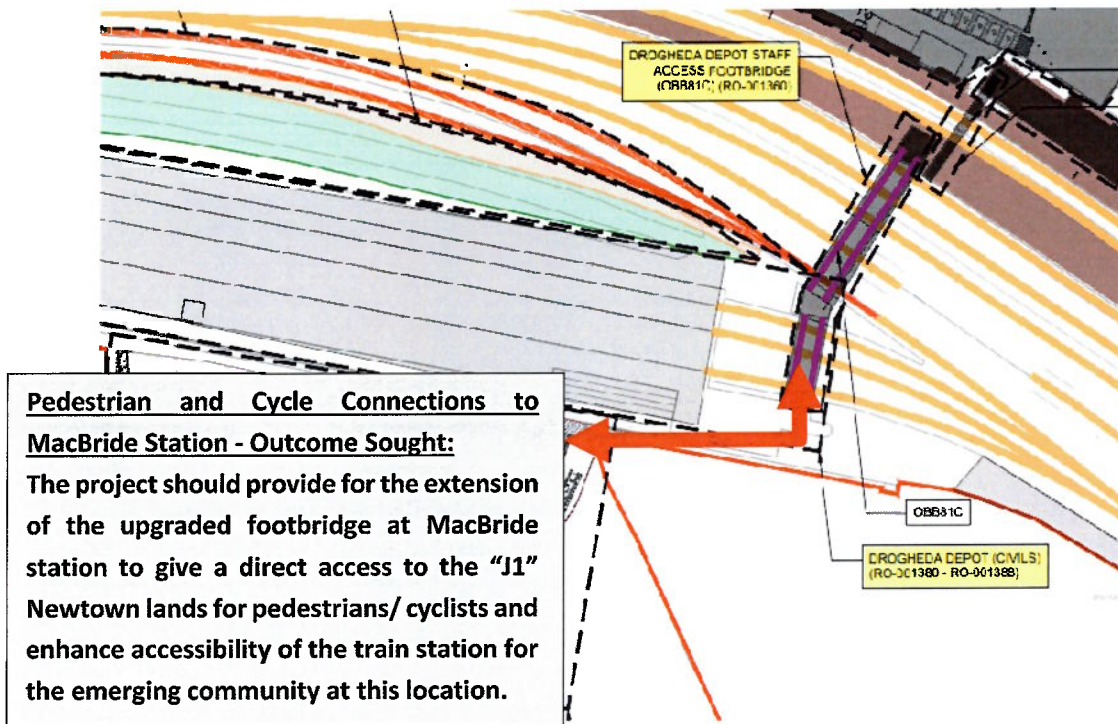


Fig 8: Suggested Provisions for Future Pedestrian/ Cycle Link to MacBride Station

6.0 TEMPORARY LAND TAKE – CONSTRUCTION COMPOUND

The subject lands are zoned and ready for development in a no-scheme world. The Murphy/ Ravala lands have services and have an access road completed which was funded and constructed by our clients. The significant up front cost of this access road and other infrastructure was to be paid for by housing which cannot now progress in the intended timeframe and the effective sterilization of practically the entirety of our clients' lands for a significant indeterminate period of time is entirely unacceptable.

This impact of the project on our clients' lands has not been adequately identified, assessed or justified in the Railway Order application and this applies to the actual location and to the scale/ extent of the construction compounds proposed.

Our clients' request that an alternative location for the compound is identified which does not impact upon their short to medium term development proposals for these lands. For example, there are other suitable lands in the area, including unzoned lands owned by Louth County Council to the east of the Newtown Access Road and west and south of the Drogheda Wastewater treatment Plant, which are convenient for the DART+ project – see lands identified as "1" on Fig 9. Other potential locations include the undeveloped industrial lands to the north off Marsh Road and the Pitch and Putt course. There are also large underutilised areas within the CIE lands around MacBride Station which could reduce the extent of the compounds required. There is no indication that the alternative options have been adequately assessed.

The extent of the temporary land take sought appears excessive and it is irregular in shape leaving the entire of the subject lands either sterilised or effectively sterilised for the period of the construction of the Dart+ Coastal North project which will remove their ability to develop the lands. The information contained within the Railway Order / EIA (4.5.11.1) in this regard is vague and unclear in respect of the compound requirements. There are no indicative layouts provided within the Railway Order to assist in understanding the extent or layout of the compound requirements and how they would impact on our clients' lands in the short to medium term.

Temporary Land Take: Construction Compounds - Outcome Sought:

Remove the temporary construction compounds from the Murphy / Ravala lands and relocate/ rationalise the extent of these areas on other lands in the vicinity that are unzoned and in public ownership.



Fig 9: Extent of Temporary Land Take and Alternative Construction Compound Locations

7.0 CONCLUSION

Based on the above assessment of the Railway Order application we request that the following Amendments are incorporated in final Railway Order or that the proposals are rejected in favour of alternative proposals that address the concerns raised.

Reconstruction of Overbridge at McGrath's Lane - Outcome Sought

The Railway Order application does not adequately accommodate the future development of the J1 'J1 (*Transportation Development Hub*)' lands at Newtown. The proposed use of embankments results in unnecessary acquisition of prime development land adjacent to a railway station node and is unsustainable and contrary to national and local planning policy. The Railway Order proposals at this location should be adjusted to include retaining walls (preferably a piled wall) to enable our client to construct the necessary high-density development up to the retaining wall and to facilitate future cycle/ pedestrian connection to the upgraded McGrath's Lane/ Railway Terrace. Refer to sketch enclosed.

Substation Compound at MacBride Station - Outcome Sought

- The proposed substation should be relocated to other lands which are more suitable. The assessment of alternative locations for the ESB Substation compound is inadequate and does not support the decision to permanently acquire the subject lands for this purpose
- The footprint and the associated land take of the substation compound is excessive and represents an inefficient use of prime development land adjacent to a railway station node suitable for high density development. Furthermore, the location and design of the substation compound will give rise to significant injurious affection on the retained lands in the vicinity of the proposed ESB station.

Pedestrian and Cycle Connections to MacBride Station - Outcome Sought

- The project should provide for the extension of the upgraded footbridge at MacBride station to give a direct access to the "J1" Newtown lands for pedestrians/ cyclists and enhance accessibility of the train station for the emerging community at this location.

Temporary Land Take: Construction Compounds - Outcome Sought

- Remove the temporary construction compounds from the Murphy / Ravala lands and relocate/ rationalise the extent of these areas on other lands in the vicinity that are unzoned and in public ownership.

We look forward to your decision in due course.

Ray Ryan
BMA Planning
October 2024

APPENDIX A

DETAILS OF LANDS TO BE ACQUIRED FROM DART+ COASTAL NORTH BOOK OF REFERENCE

PERMANENT ACQUISITIONS

DART- COASTAL NORTH - BOOK OF REFERENCE - SECOND SCHEDULE (PART 1)			Property Plan	5038
Land which may be acquired			Property No	P.1(A)
Quantity, description, and situation of land	OWNERS OR REPUTED OWNERS	LESSEES OR REPUTED LESSEES	OCCUPIERS	
Quantity (sq.m.) 1 909.91 Description Development Lands Townland Newtown Situation McGraths Lane, Newtown, Drogheda, Co Louth	J Murphy (Developments) Limited Block D Brvaanstown Centre, Dublin Road Drogheda Louth	None	Owner(s)	
Observations Refer to Works Layout 22, 23, 24 and 25 for information on the works in this area.			Ref By BL Date 21/06/2024 Ref No 5038.P.1(A)	

DART- COASTAL NORTH - BOOK OF REFERENCE - SECOND SCHEDULE (PART 1)			Property Plan	5038
Land which may be acquired			Property No	P.7(A)
Quantity, description, and situation of land	OWNERS OR REPUTED OWNERS	LESSEES OR REPUTED LESSEES	OCCUPIERS	
Quantity (sq.m.) 3 806.43 Description Development Lands Townland Lagavoreen Situation McGraths Lane, Newtown, Drogheda, Co Louth	Ravala Limited 22A Dover Business Park, Greenhills Drogheda Louth	None	Owner(s)	
Observations Refer to Works Layout 22, 23, 24 and 25 for information on the works in this area.			Ref By BL Date 24/06/2024 Ref No 5038.P.7(A)	

TEMPORARY ACQUISITIONS

DART+ COASTAL NORTH – BOOK OF REFERENCE – THIRD SCHEDULE
 Land of which temporary possession may be taken

Property Plan	5038
Property No	T.1(A)

Quantity, description, and situation of land	OWNERS OR REPUTED OWNERS	LESSEES OR REPUTED LESSEES	OCCUPIERS
Quantity (sq.m.) 43,827.77 Description Developmental Leads Townland Newtown Situation McGraths Lane, Newtown, Drogheda, Co. Louth	J Murphy (Developments) Limited Block B Bryanstown Centre, Dublin Road, Drogheda, Louth	None	Owner(s)

Observations
 Refer to Works Layout 22, 23, 24 and 25 for information on the works in this area.

Ref By	DL
Date	21/06/2021
Ref No	5038 T.1(A)

DART+ COASTAL NORTH – BOOK OF REFERENCE - THIRD SCHEDULE
 Land of which temporary possession may be taken

Property Plan	5038
Property No	1.7(A)

Quantity, description, and situation of land	OWNERS OR REPUTED OWNERS	LESSEES OR REPUTED LESSEES	OCCUPIERS
Quantity (sq.m.) 12,522.61 Description Development Lands Townland Lagavocree Situation McGraths Lane, Newtown, Drogheda, Co. Louth	Ravala Limited 22A Bowne Business Park Greeshills, Drogheda, Louth	None	Owner(s)

Observations
 Refer to Works Layout 22, 23, 24 and 25 for information on the works in this area.

Ref By	DL
Date	21/06/2021
Ref No	5038 T.7(A)

PERMANENT AND TEMPORARY RIGHTS OF WAY AND EASEMENTS

DART+ COASTAL NORTH - BOOK OF REFERENCE - FOURTH SCHEDULE (PART 1) Land over which Private Rights of Way or Other Easements may be acquired			Property Plan	5038
			Property No	4P.1(A)
Quantity, description, and situation of land	OWNERS OR REPUTED OWNERS	LESSEES OR REPUTED LESSEES	OCCUPIERS	
Quantity (sq.m) 1.451.63 Description Development Lands Townland Newtown Situation McGraths Lane Newtown, Drogheda, Co. Louth	J. Murphy (Developments) Limited Block D Bryanstown Centre, Duslin Road, Drogheda, Louth	None	Owner(s)	
Observations Refer to Works Layouts 22, 23, 24 and 25 for information on the works in this area.			Ref By	BL
			Date	24/06/2024
			Ref No	5038.4P.1(A)

DART+ COASTAL NORTH - BOOK OF REFERENCE - FOURTH SCHEDULE (PART 1) Land over which Private Rights of Way or Other Easements may be acquired			Property Plan	5038
			Property No	4P.1(B)
Quantity, description, and situation of land	OWNERS OR REPUTED OWNERS	LESSEES OR REPUTED LESSEES	OCCUPIERS	
Quantity (sq.m) 88.17 Description Development Lands Townland Newtown Situation McGraths Lane, Newtown, Drogheda, Co. Louth	J. Murphy (Developments) Limited Block B Bryanstown Centre, Dublin Road, Drogheda, Louth	None	Owner(s)	
Observations Refer to Works Layouts 22, 23, 24 and 25 for information on the works in this area.			Ref By	BL
			Date	24-06-2024
			Ref No	5038.4P.1(B)

DART COASTAL NORTH – BOOK OF REFERENCE - FOURTH SCHEDULE (PART 2)
Land over which Temporary Private Rights of Way or Other Easements may be acquired

Property Plan	5038
Property No	41.1(A)

Quantity, description, and situation of land	OWNERS OR REPUTED OWNERS	TESSEES OR REPUTED TESSSES	OCCUPIERS
Quantity (sq.m.) 6.535.55 Description Development: Lands Townland Newtown Situation McGaule Lane, Newtown Drogheda, Co. Louth	J. Murphy (Developments) Limited Block B Droyastown Centre, Dublin Road, Drogheda, Louth	None	Owner(s)

Observations
 Refer to Works Layout 22, 23, 24 and 25 for information on the works in this area.

Ref By	DL
Date	24/06/2024
Ref No	5038.4T.1(A)